

MULTIMODAL COMMUNITY PLANNING STUDY

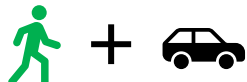

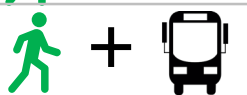
STREET CROSS SECTIONS PACKAGE
JULY 2019

 **KITTELSON**
& ASSOCIATES



 **NEXT STOP**
FORT LAUDERDALE
Advancing the Vision

Street Section Prototypes

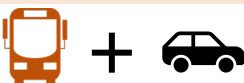
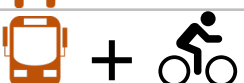

Pedestrian Priority Streets

- *Ped Priority + Auto* 
- *Ped Priority + Bike* 
- *Ped Priority + Transit* 



Bike Priority Streets

- *Bike Priority + Auto* 
- *Bike Priority + Ped* 

Transit Priority Streets

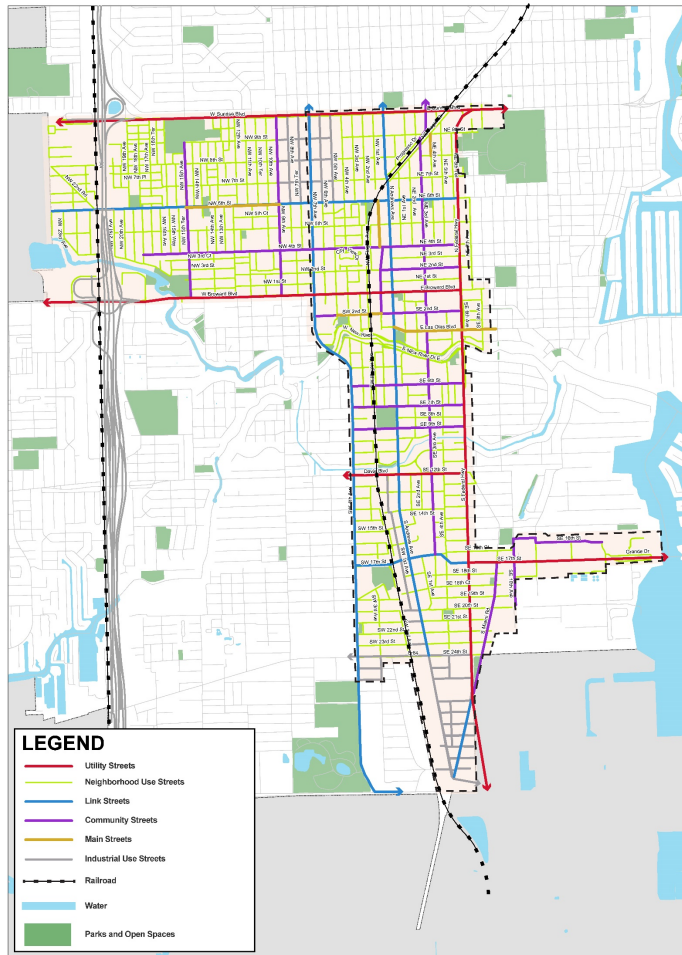
- *Transit Priority + Auto* 
- *Transit Priority + Bike* 
- *Transit Priority + Ped* 

Auto Priority Streets

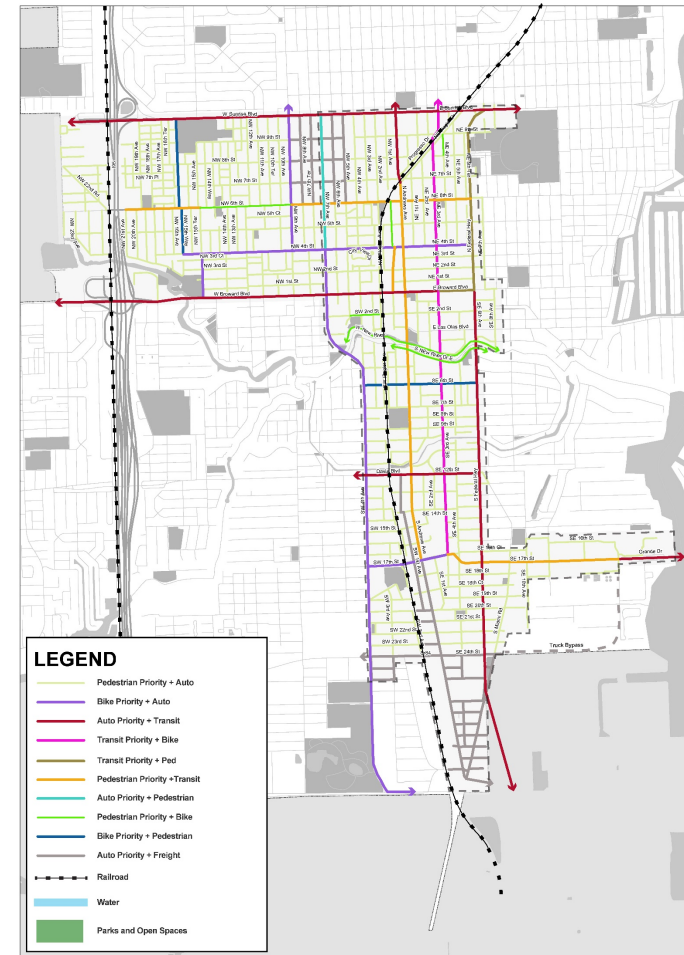
- *Auto Priority + Ped* 
- *Auto Priority + Freight* 

Street Section Prototypes

- The street section prototypes in this document are to be utilized as a design guide for streets within the planning area. Assumptions and criteria were based on analysis and discussions held as part of the planning process. Please use the Pedestrian Use and Modal Priority Maps for reference.



Multimodal Community Planning Study - Pedestrian Use Map

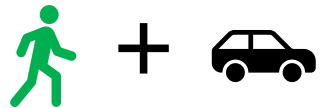


Multimodal Community Planning Study- Modal Priority Map

Option #1



**Lower Density Context*



Pedestrian Priority + Auto Street Prototype

Typical Neighborhood Use Street: 40' ROW minimum*

**final widths may vary*

Sample Street: NW 3rd Street



Neighborhood Use Street

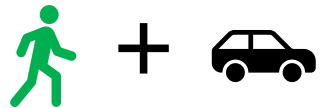
QUANTITATIVE MEASURES				
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT
Inter-community trip		Family/residents	1-2	Residential

QUALITATIVE MEASURES	
ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 1-2 Median presence: NO Curb and Gutter: Maybe Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' (MINIMUM) Driveway Use: LOW Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Presence of trees / shade Continuous and unobstructed sidewalk

Option #2



**Lower Density Context*



Pedestrian Priority + Auto Street Prototype

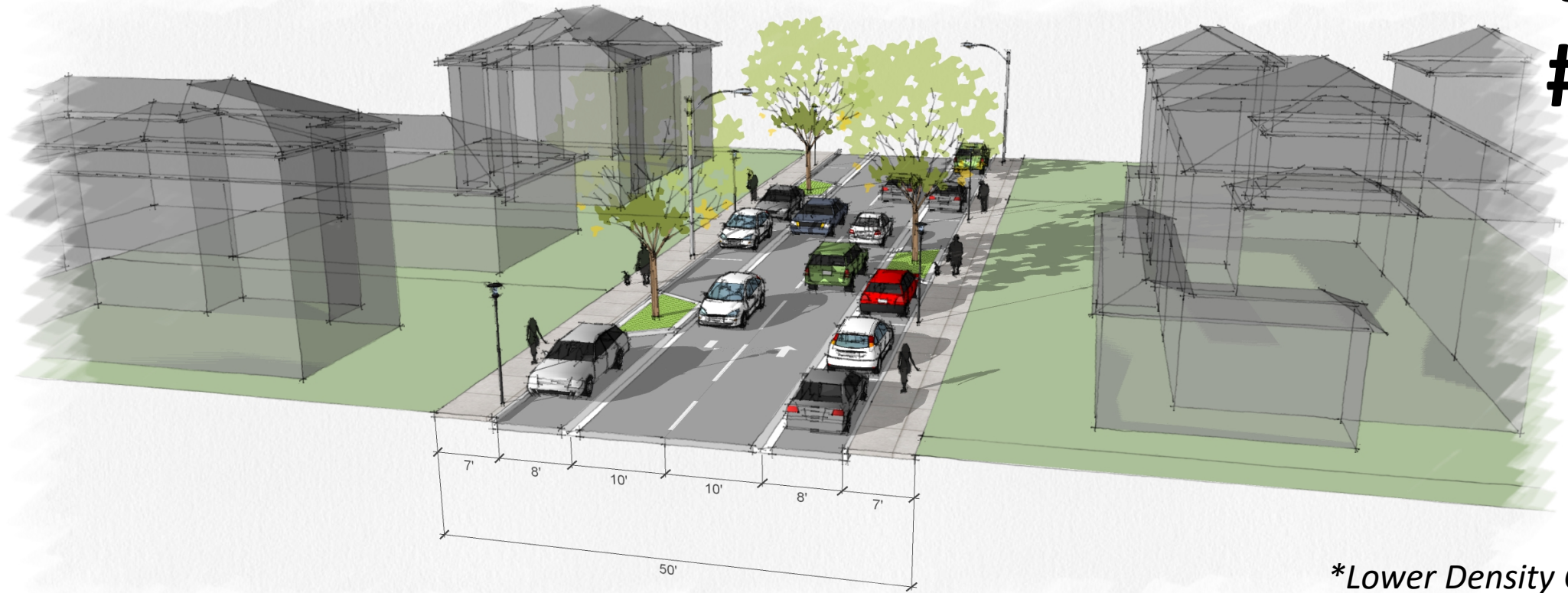
Typical Neighborhood Use Street: 50' ROW minimum*

**final widths may vary*

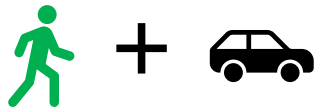
Sample Street: NW 3rd Street

		QUANTITATIVE MEASURES					QUALITATIVE MEASURES	
		EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK	ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
<p>Neighborhood Use Street</p>	TRIP TYPE	Inter-community trip						
			Family/ residents	1-2	Residential	Up to 25 feet	Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 1-2 Median presence : NO Curb and Gutter: Maybe Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' (MINIMUM) Driveway Use: LOW Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Presence of trees / shade Continuous and unobstructed sidewalk

Option #3A



**Lower Density Context*



Pedestrian Priority + Auto Street Prototype

Typical Neighborhood Use Street: 50' ROW minimum*

**final widths may vary*

Sample Street: NW 3rd Street

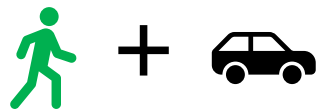


QUANTITATIVE MEASURES					QUALITATIVE MEASURES		
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK	ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Inter-community trip		Family/ residents	1-2	Residential	Up to 25 feet	Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 1-2 Median presence: NO Curb and Gutter: Maybe Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' (MINIMUM) Driveway Use: LOW Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Presence of trees / shade Continuous and unobstructed sidewalk

Option #3B



**Higher Density Context*



Pedestrian Priority + Auto Street Prototype

Typical Neighborhood Use Street: 50' ROW minimum*

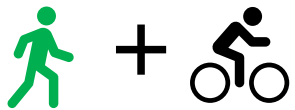
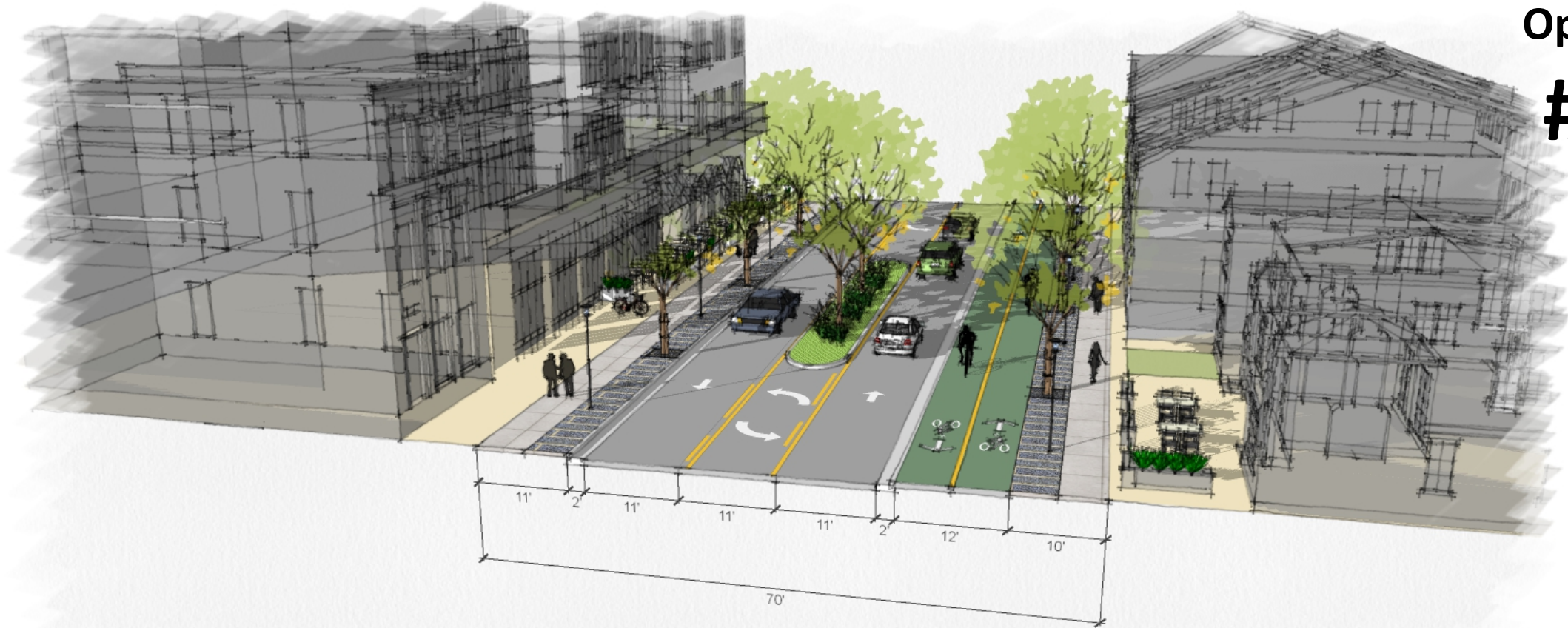
**final widths may vary*

Sample Street: NE 3rd Street



QUANTITATIVE MEASURES					QUALITATIVE MEASURES		
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK	ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Inter-community trip		Family/ residents	1-2	Residential	Up to 25 feet	Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 1-2 Median presence : NO Curb and Gutter: Maybe Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' (MINIMUM) Driveway Use: LOW Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Presence of trees / shade Continuous and unobstructed sidewalk

Option #1



Pedestrian Priority + Bike Street Prototype
Typical Main Street: 70' ROW minimum*
**final widths may vary*
 Sample Street: SW 2nd Street



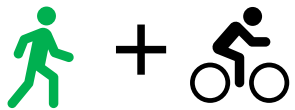
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Leisure/entertainment		Visitors, families, transit users, residents	N/A	Mixed-used/commercial	Up to 15 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2-4 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: ON BOTH SIDES Sidewalk width: >12' Driveway Use: HIGH Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): YES 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian scaled streetscape elements Awnings Buildings up to the street Active groundfloor On-street parking High emphasis crosswalks at every intersection Pick-up / drop-off zones

Option #2

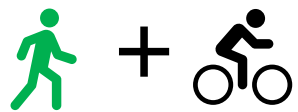


Pedestrian Priority + Bike Street Prototype
Typical Main Street: 70' ROW minimum*
 *final widths may vary
 Sample Street: SW 2nd Street



	QUANTITATIVE MEASURES					QUALITATIVE MEASURES	
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK	ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Leisure/entertainment		Visitors, families, transit users, residents	N/A	Mixed-used/commercial	Up to 15 feet	Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2-4 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: ON BOTH SIDES Sidewalk width: >12' Driveway Use: HIGH Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): YES 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian scaled streetscape elements Awnings Buildings up to the street Active groundfloor On-street parking High emphasis crosswalks at every intersection Pick-up / drop-off zones

Option #3



Pedestrian Priority + Bike Street Prototype
Typical Main Street: 70' ROW minimum*
 *final widths may vary
 Sample Street: SW 2nd Street

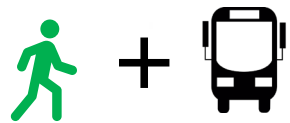


	QUANTITATIVE MEASURES					QUALITATIVE MEASURES	
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK	ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Leisure/entertainment		Visitors, families, transit users, residents	N/A	Mixed-used/commercial	Up to 15 feet	Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2-4 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: ON BOTH SIDES Sidewalk width: >12' Driveway Use: HIGH Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): YES 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian scaled streetscape elements Awnings Buildings up to the street Active groundfloor On-street parking High emphasis crosswalks at every intersection Pick-up / drop-off zones

Option #1



**Upon redevelopment of properties, additional space to be dedicated to the sidewalk zone (as shown in option #4)*



Pedestrian Priority + Transit Street Prototype

Typical Link Street: 70' ROW minimum*

**final widths may vary*

Sample Street: Andrews Ave. North of Davie Blvd.



TRIP TYPE

Mix of community connections and commuter trips

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE
Family/residents, transit users, employees/workers

LEVEL OF COMFORT (LTS)

2-3

LAND USE CONTEXT
Mostly commercial/office/institutional uses

BUILDING SETBACK

Up to 60 feet

QUANTITATIVE MEASURES

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS

- Travelway
 - On-street parking: MAYBE
 - Total number of travel lanes: 3-4
 - Median presence: MAYBE
 - Curb and Gutter: YES
 - Bike Parking: YES
- Street Side
 - Sidewalk presence: On Both Sides
 - Sidewalk width: 5'-12'
 - Driveway Use: MEDIUM
 - Street Trees: YES
 - Street Furnishings (pedestrian scale lighting, furnishings etc.): YES

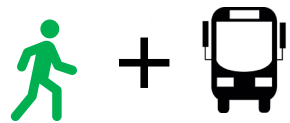
KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities
- Bus stops/Shelters

Option #2



**Upon redevelopment of properties, additional space to be dedicated to the sidewalk zone (as shown in option #4)*



Pedestrian Priority + Transit Street Prototype

Typical Link Street: 70' ROW minimum*

**final widths may vary*

Sample Street: Andrews Ave. North of Davie Blvd.



TRIP TYPE

Mix of community connections and commuter trips

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE

Family/residents, transit users, employees/workers

LEVEL OF COMFORT (LTS)

2-3

LAND USE CONTEXT

Mostly commercial/office/institutional uses

BUILDING SETBACK

Up to 60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS

- Travelway
 - On-street parking: MAYBE
 - Total number of travel lanes: 3-4
 - Median presence: MAYBE
 - Curb and Gutter: YES
 - Bike Parking: YES
- Street Side
 - Sidewalk presence: On Both Sides
 - Sidewalk width: 5'-12'
 - Driveway Use: MEDIUM
 - Street Trees: YES
 - Street Furnishings (pedestrian scale lighting, furnishings etc.): YES

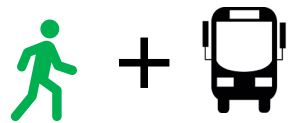
KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities
- Bus stops/Shelters

Option #3



**Upon redevelopment of properties, additional space to be dedicated to the sidewalk zone (as shown in option #4)*



Pedestrian Priority + Transit Street Prototype

Typical Link Street: 70' ROW minimum*

**final widths may vary*

Sample Street: Andrews Ave. North of Davie Blvd.



TRIP TYPE

Mix of community connections and commuter trips

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE
Family/residents, transit users, employees/workers

LEVEL OF COMFORT (LTS)
2-3

LAND USE CONTEXT
Mostly commercial/office/institutional uses

BUILDING SETBACK
Up to 60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS

- Travelway
 - On-street parking: MAYBE
 - Total number of travel lanes: 3-4
 - Median presence: MAYBE
 - Curb and Gutter: YES
 - Bike Parking: YES
- Street Side
 - Sidewalk presence: On Both Sides
 - Sidewalk width: 5'-12'
 - Driveway Use: MEDIUM
 - Street Trees: YES
 - Street Furnishings (pedestrian scale lighting, furnishings etc.): YES

KEY ELEMENTS OF SUCCESS

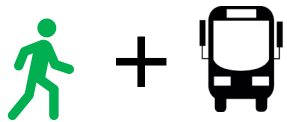
- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities
- Bus stops/Shelters

Option #4



**This option illustrates an expanded ROW width as provided by the Trafficways Plan (88 ft)*

**Upon redevelopment of properties, additional space to be dedicated to the sidewalk zone (as shown in this option)*



Pedestrian Priority + Transit Street Prototype

Typical Link Street: 70' ROW minimum*

**final widths may vary*

Sample Street: Andrews Ave. North of Davie Blvd.



TRIP TYPE

Mix of community connections and commuter trips

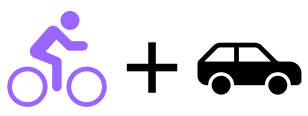
QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Family/residents, transit users, employees/workers	2-3	Mostly commercial/office/institutional uses	Up to 60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 3-4 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5'-12' Driveway Use: MEDIUM Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): YES 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian-scaled lighting Awnings Sidewalk on both sides Access to community amenities Bus stops/Shelters

Option #1



Bike Priority + Auto Street Prototype

Typical Community Street: 60' ROW minimum*

**final widths may vary*

Sample Street: NW 4th Street



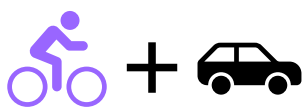
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Mix of trips to commercial/community amenities and daily commuting		Family/residents	2-3	Mostly mix of residential and	Up to 40 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5'-12' Driveway Use: Medium Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian-scaled lighting Awnings Sidewalk on both sides Access to community amenities

Option #2



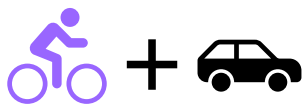
Bike Priority + Auto Street Prototype
Typical Community Street: 60' ROW minimum*
**final widths may vary*
 Sample Street: NW 4th Street



TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Mix of trips to commercial/community amenities and daily commuting		Family/residents	2-3	Mostly mix of residential and	Up to 40 feet

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5'-12' Driveway Use: Medium Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian-scaled lighting Awnings Sidewalk on both sides Access to community amenities

Option #3



Bike Priority + Auto Street Prototype

Typical Community Street: 60' ROW minimum*

**final widths may vary*

Sample Street: NW 4th Street



TRIP TYPE

Mix of trips to commercial/community amenities and daily commuting



Family/residents

LEVEL OF COMFORT (LTS)
2-3

LAND USE CONTEXT
Mostly mix of residential and

BUILDING SETBACK
Up to 40 feet

QUANTITATIVE MEASURES

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS

- Travelway
- On-street parking: YES
- Total number of travel lanes: 2
- Median presence: MAYBE
- Curb and Gutter: YES
- Bike Parking: YES
- Street Side
- Sidewalk presence: On Both Sides
- Sidewalk width: 5'-12'
- Driveway Use: Medium
- Street Trees: YES
- Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE

KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities

Option #1



**Higher Density Context*



Bike Priority + Pedestrian Street Prototype

Typical Community Use Street: 50' ROW minimum*

**final widths may vary*

Sample Street: SW 6th Street



TRIP TYPE
Mix of trips to commercial/community amenities and daily commuting

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Family/residents	2-3	Mostly mix of residential and	Up to 40 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway • On-street parking: YES • Total number of travel lanes: 2 • Median presence: MAYBE • Curb and Gutter: YES • Bike Parking: YES Street Side • Sidewalk presence: On Both Sides • Sidewalk width: 5'-12' • Driveway Use: Medium • Street Trees: YES • Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE	• Presence of trees / shade • Pedestrian-scaled lighting • Awnings • Sidewalk on both sides • Access to community amenities

Option #1A



**Lower Density Context*



Bike Priority + Pedestrian Street Prototype
Typical Community Use Street: 50' ROW minimum*
**final widths may vary*
 Sample Street: SW 6th Street



TRIP TYPE
 Mix of trips to commercial/community amenities and daily commuting

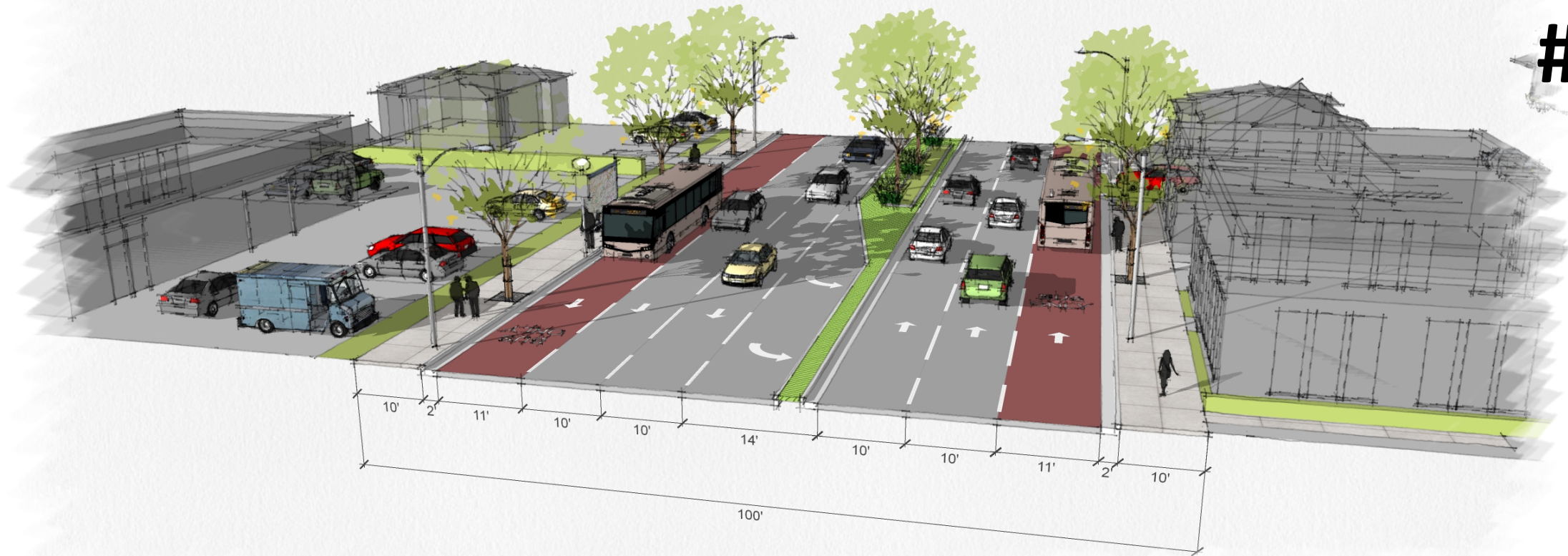
QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Family/residents	2-3	Mostly mix of residential and	Up to 40 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway • On-street parking: YES • Total number of travel lanes: 2 • Median presence: MAYBE • Curb and Gutter: YES • Bike Parking: YES Street Side • Sidewalk presence: On Both Sides • Sidewalk width: 5'-12' • Driveway Use: Medium • Street Trees: YES • Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE	• Presence of trees / shade • Pedestrian-scaled lighting • Awnings • Sidewalk on both sides • Access to community amenities

Option #1



Transit Priority + Auto Street Prototype
Typical Utility Street: 100' ROW minimum*
**final widths may vary*
 Sample Street: Broward Blvd



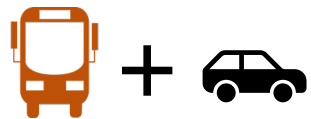
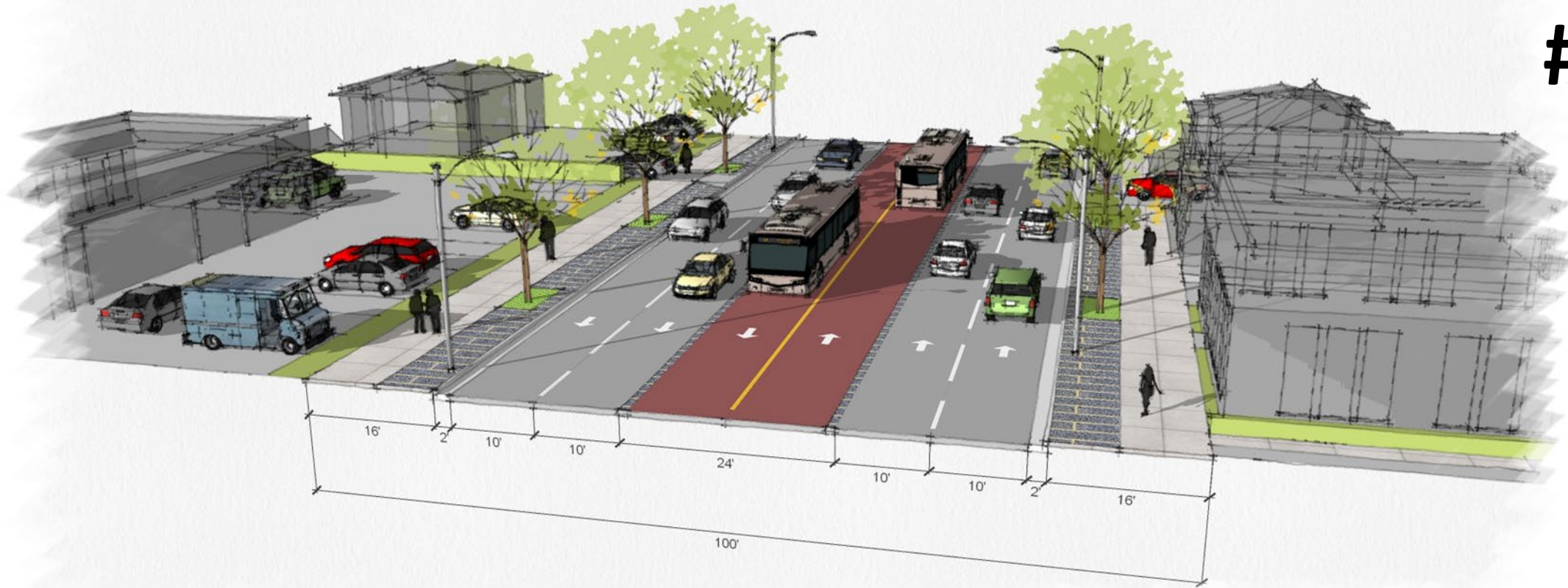
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Transit connection for commuter/regional trips		Commuters	4	Single land use/stand-alone commercial	>60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: NO Total number of travel lanes: 4-7 Median presence: MAYBE Curb and Gutter: YES Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5' Driveway Use: High Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Minimum standard sidewalks Bus stops/Shelters Safe pedestrian crossing opportunities Lighting

Option #2



Transit Priority + Auto Street Prototype
Typical Utility Street: 100' ROW minimum*
**final widths may vary*
 Sample Street: Broward Blvd



Utility Street

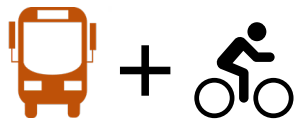
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Transit connection for commuter/regional trips	5	Commuters	4	Single land use/stand-alone commercial	>60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: NO Total number of travel lanes: 4-7 Median presence: MAYBE Curb and Gutter: YES Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5' Driveway Use: High Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Minimum standard sidewalks Bus stops/Shelters Safe pedestrian crossing opportunities Lighting

Option #1



Transit Priority + Bike Street Prototype

Typical Community Street: 70' ROW minimum*

**final widths may vary*

Sample Street: SE 3rd Ave



TRIP TYPE

Mix of trips to commercial/community amenities and daily commuting

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE

Family/residents

LEVEL OF COMFORT (LTS)

2-3

LAND USE CONTEXT

Mostly mix of residential and

BUILDING SETBACK

Up to 40 feet

QUALITATIVE MEASURES

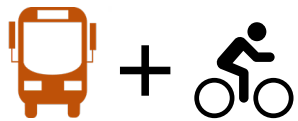
ROADWAY CHARACTERISTICS

- On-street parking: YES
- Total number of travel lanes: 2
- Median presence: MAYBE
- Curb and Gutter: YES
- Bike Parking: YES
- Street Side
 - Sidewalk presence: On Both Sides
 - Sidewalk width: 5'-12'
 - Driveway Use: Medium
 - Street Trees: YES
 - Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE

KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities

Option #2



Transit Priority + Bike Street Prototype

Typical Community Street: 70' ROW minimum*

**final widths may vary*

Sample Street: SE 3rd Ave



TRIP TYPE

Mix of trips to commercial/community amenities and daily commuting

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE

Family/residents

LEVEL OF COMFORT (LTS)

2-3

LAND USE CONTEXT

Mostly mix of residential and

BUILDING SETBACK

Up to 40 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS

- On-street parking: YES
- Total number of travel lanes: 2
- Median presence: MAYBE
- Curb and Gutter: YES
- Bike Parking: YES

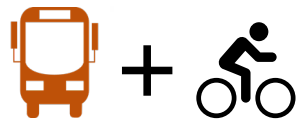
Street Side

- Sidewalk presence: On Both Sides
- Sidewalk width: 5'-12'
- Driveway Use: Medium
- Street Trees: YES
- Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE

KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities

Option #3



Transit Priority + Bike Street Prototype

Typical Community Street: 70' ROW minimum*

**final widths may vary*

Sample Street: SE 3rd Ave



TRIP TYPE
Mix of trips to commercial/community amenities and daily commuting

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Family/residents	2-3	Mostly mix of residential and	Up to 40 feet

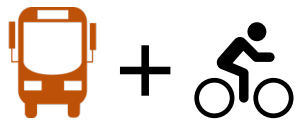
QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
<p>Travelway</p> <ul style="list-style-type: none"> On-street parking: YES Total number of travel lanes: 2 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES <p>Street Side</p> <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5'-12' Driveway Use: Medium Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian-scaled lighting Awnings Sidewalk on both sides Access to community amenities

Option #4



**This option illustrates an expanded ROW width as provided by the Trafficways Plan (80 ft.)*



Transit Priority + Bike Street Prototype

Typical Community Street: 70' ROW minimum*

**final widths may vary*

Sample Street: SE 3rd Ave



TRIP TYPE

Mix of trips to commercial/community amenities and daily commuting

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME



PEDESTRIAN TYPE

Family/residents

LEVEL OF COMFORT (LTS)

2-3

LAND USE CONTEXT

Mostly mix of residential and

BUILDING SETBACK

Up to 40 feet

QUALITATIVE MEASURES

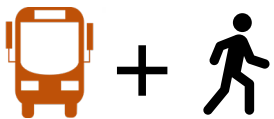
ROADWAY CHARACTERISTICS

- On-street parking: YES
- Total number of travel lanes: 2
- Median presence: MAYBE
- Curb and Gutter: YES
- Bike Parking: YES
- Street Side
 - Sidewalk presence: On Both Sides
 - Sidewalk width: 5'-12'
 - Driveway Use: Medium
 - Street Trees: YES
 - Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE

KEY ELEMENTS OF SUCCESS

- Presence of trees / shade
- Pedestrian-scaled lighting
- Awnings
- Sidewalk on both sides
- Access to community amenities

Option #1



Transit Priority + Auto Street Prototype
Typical Utility Street: 100' ROW minimum*
**final widths may vary*
 Sample Street: N. Federal Hwy



TRIP TYPE

Transit connection for commuter/regional trips

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Commuters	4	Single land use/stand-alone commercial	>60 feet

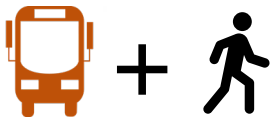
QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway • On-street parking: NO • Total number of travel lanes: 4-7 • Median presence: MAYBE • Curb and Gutter: YES • Bike Parking: NO Street Side • Sidewalk presence: On Both Sides • Sidewalk width: 5' • Driveway Use: High • Street Trees: MAYBE • Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE	• Minimum standard sidewalks • Bus stops/Shelters • Safe pedestrian crossing opportunities • Lighting

Option #2



**This option illustrates an expanded ROW width as provided by the Trafficways Plan (120 ft.)*



Transit Priority + Auto Street Prototype
Typical Utility Street: 100' ROW minimum*
**final widths may vary*
 Sample Street: N. Federal Hwy



TRIP TYPE

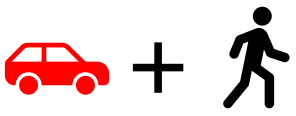
Transit connection for commuter/regional trips

QUANTITATIVE MEASURES

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Commuters	4	Single land use/stand-alone commercial	>60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
<p>Travelway</p> <ul style="list-style-type: none"> On-street parking: NO Total number of travel lanes: 4-7 Median presence: MAYBE Curb and Gutter: YES Bike Parking: NO <p>Street Side</p> <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5' Driveway Use: High Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): MAYBE 	<ul style="list-style-type: none"> Minimum standard sidewalks Bus stops/Shelters Safe pedestrian crossing opportunities Lighting



Auto Priority + Pedestrian Street Prototype
Typical Link Street: 90' ROW minimum*
 *final widths may vary
 Sample Street: NW 7th Avenue

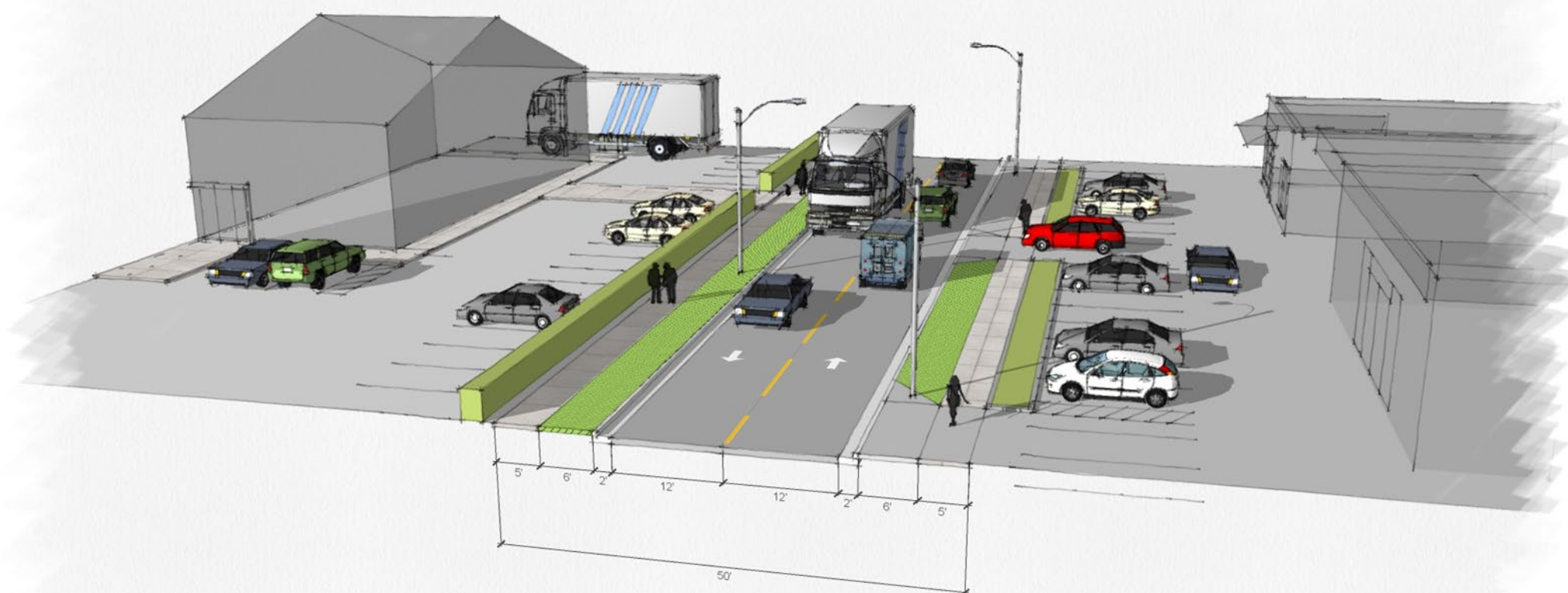


TRIP TYPE
 Mix of community connections and commuter trips

EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
	Family/residents, transit users, employees/workers	2-3	Mostly commercial/office/institutional uses	Up to 60 feet

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 3-4 Median presence: MAYBE Curb and Gutter: YES Bike Parking: YES Street Side <ul style="list-style-type: none"> Sidewalk presence: On Both Sides Sidewalk width: 5'-12' Driveway Use: MEDIUM Street Trees: YES Street Furnishings (pedestrian scale lighting, furnishings etc.): YES 	<ul style="list-style-type: none"> Presence of trees / shade Pedestrian-scaled lighting Awnings Sidewalk on both sides Access to community amenities Bus stops/Shelters

Option #1



Auto Priority + Freight Street Prototype

Typical Industrial Use Street: 50' ROW minimum*

**final widths may vary*

Sample Street: SW 1st Ave



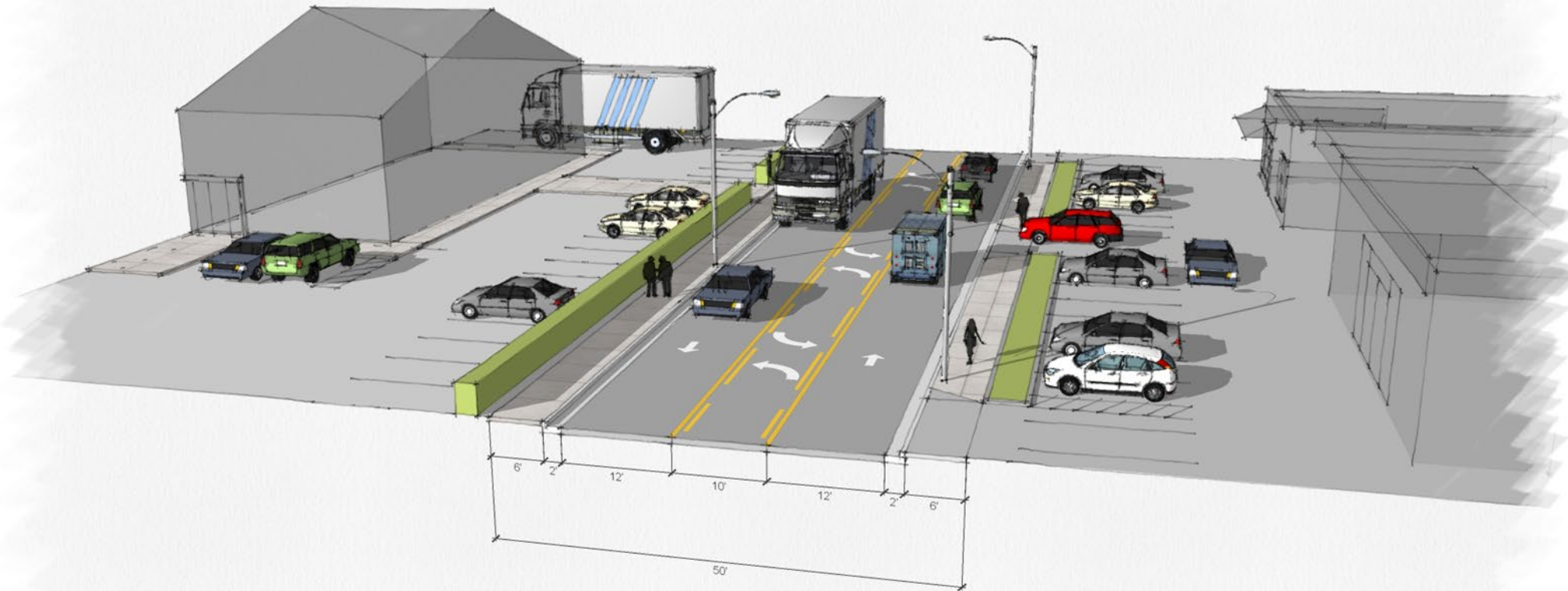
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Industrial work trips		Limited Use	4	Industrial	>60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
<p>Travelway</p> <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 2-4 Median presence: NO Curb and Gutter: MAYBE Bike Parking: NO <p>Street Side</p> <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' Driveway Use: Medium Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Minimum Standard Sidewalk

Option #2



Auto Priority + Freight Street Prototype

Typical Industrial Use Street: 50' ROW minimum*

**final widths may vary*

Sample Street: SW 1st Ave



Industrial Street

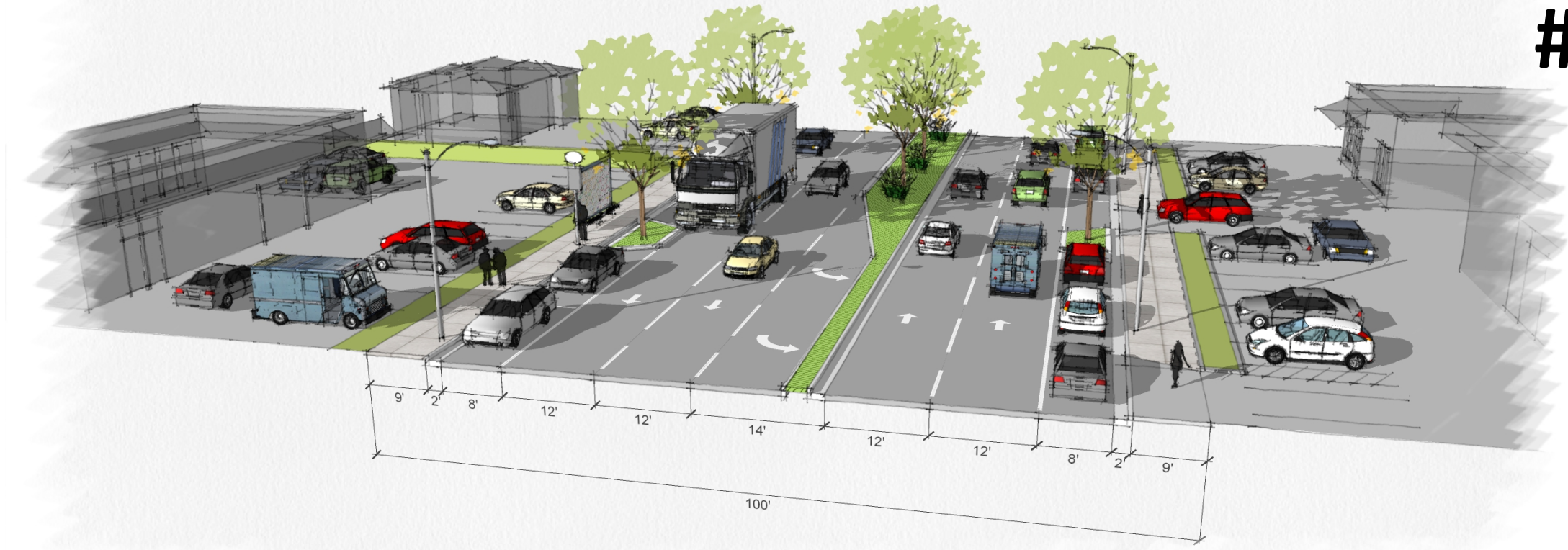
QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
Industrial work trips		Limited Use	4	Industrial	>60 feet

QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 2-4 Median presence: NO Curb and Gutter: MAYBE Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' Driveway Use: Medium Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Minimum Standard Sidewalk

Option #3



Auto Priority + Freight Street Prototype
Typical Industrial Use Street: 100' ROW minimum*
 *final widths may vary
 Sample Street: SE 24th Street



QUANTITATIVE MEASURES

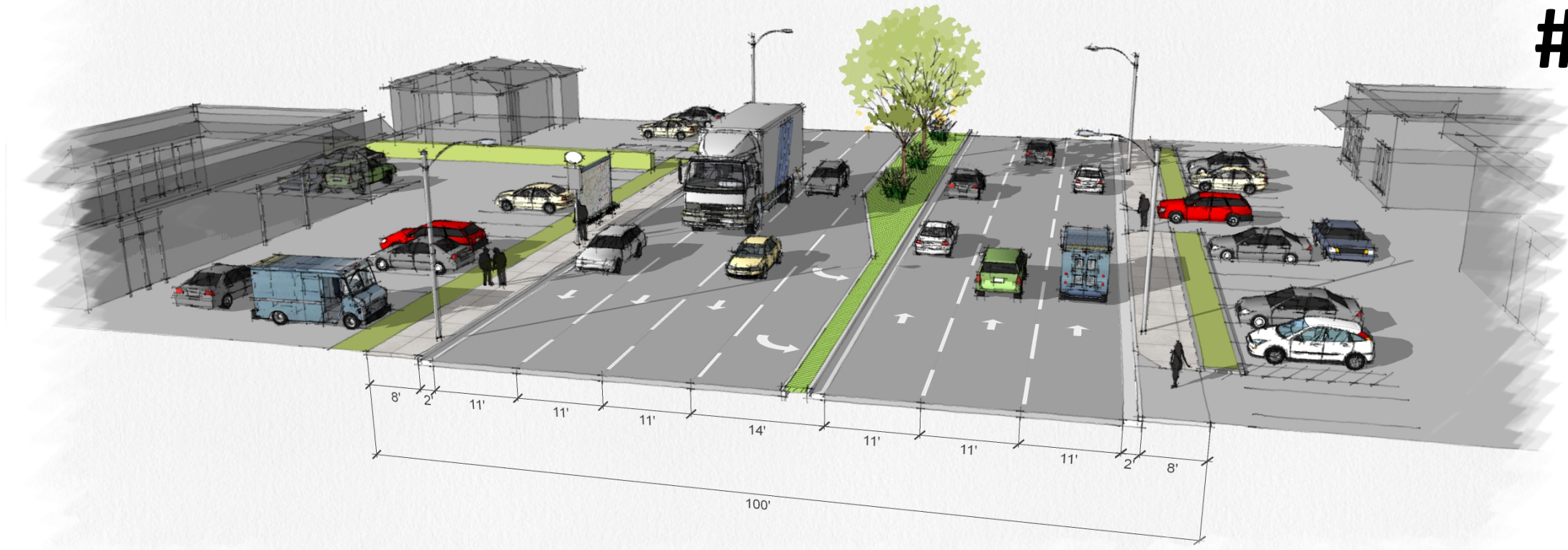
TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
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Industrial work trips		Limited Use	4	Industrial	>60 feet
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QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 2-4 Median presence: NO Curb and Gutter: MAYBE Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' Driveway Use: Medium Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Minimum Standard Sidewalk

Option #4



Auto Priority + Freight Street Prototype
Typical Industrial Use Street: 100' ROW minimum*
 *final widths may vary
 Sample Street: SE 24th Street



QUANTITATIVE MEASURES

TRIP TYPE	EXPECTED PEDESTRIAN VOLUME	PEDESTRIAN TYPE	LEVEL OF COMFORT (LTS)	LAND USE CONTEXT	BUILDING SETBACK
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Industrial work trips		Limited Use	4	Industrial	>60 feet
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QUALITATIVE MEASURES

ROADWAY CHARACTERISTICS	KEY ELEMENTS OF SUCCESS
Travelway <ul style="list-style-type: none"> On-street parking: MAYBE Total number of travel lanes: 2-4 Median presence: NO Curb and Gutter: MAYBE Bike Parking: NO Street Side <ul style="list-style-type: none"> Sidewalk presence: At least on one side Sidewalk width: 5' Driveway Use: Medium Street Trees: MAYBE Street Furnishings (pedestrian scale lighting, furnishings etc.): NO 	<ul style="list-style-type: none"> Minimum Standard Sidewalk